

Laxton Parish Council Correspondence Record

19 July to 8th August 2017

The Clerk will circulate correspondence when considered appropriate. If Councillors would like to see a copy of correspondence that has not been circulated, please notify the Clerk on laxtonpc@btinternet.com or 07932 016856.

Ref	Date Received	Attached?	From	Purpose of Correspondence	Additional Information
380	19 July	Y	Environment Agency	July Newsletter	
381	26 July	N	Humberside Police	Burglaries in Holme Upon Spalding Moor.	Please remind everyone to be extra vigilant, ensuring that all car, house doors, windows and garages/sheds are securely locked and to report any suspicious people or vehicles to the Police on the non emergency number 101 or 999 in an emergency or if a crime is occurring.
382	1 August	N	ERYC – Adult Safeguarding Manager	Looking to raise awareness of Adult Safeguarding. Offer to attend any meeting, briefing or informal gathering to provide an update on the work of the Safeguarding Adults Board and how we are working to safeguard adults at risk of harm.	
383	1 August	N	ERYC – Neighbourhood Watch Liaison	Paperwork for creation of Parish Neighbourhood Watch Scheme	

Humber – keeping you informed

July 2017

This newsletter provides an update on the Comprehensive Review of the Humber Strategy and on work taking place or planned to help reduce the risk of flooding to people and properties around the Humber Estuary.

Comprehensive Review of the Humber Strategy

Work is now underway on the Comprehensive Review of the Humber Strategy, with the support of our consultants CH2M, along with other leading industry experts. This important piece of work will redefine the strategic approach to managing tidal flood risk around the estuary, setting the way forward for the next 100 years. It provides us with the opportunity to reconsider the economic, social and environmental needs of the estuary and set an adaptive flood risk management investment programme. Defra will sign-off the revised Humber Strategy in 2019.

We have extended the boundary for the review to reach further upstream from the main estuary to include the rivers where tidal flow is the main source of flood risk.

Our Project Manager's perspective

'The Comprehensive Review is critical in helping the communities in and around the Humber manage their long-term flood risk and provides a springboard for future growth and environmental enhancement in the region.

We have divided the review into different work-streams, involving internal and external experts, to ensure the review of the strategy is a success, delivering adaptable solutions for communities and businesses while considering the needs of the environment.

We are working closely with our local authority partners on the review and we really want to involve a wide range of stakeholders through the process – to seek support, ideas and innovative solutions to managing flood risk. We are currently looking at how we can do this most effectively, and we will keep you updated about how you may be able to help deliver this vitally important piece of work'



*Richard Hartley,
Environment Agency Project Manager*

If you have any questions about the review or have any initial ideas about how you could get involved, then please contact HStrategy@environment-agency.gov.uk

Scheme Updates

Keadby Pumping Station: Remedial work to sustain the operation of Keadby Pumping Station for the next 5 years is ongoing, while we continue to refine the options for a large scale investment in a replacement terminal outfall. We have now completed works to replace the old automated weed screen cleaner and all works should be completed by the end of the summer.



Water for Wildlife and Farmers: This exciting partnership project investigates how techniques used in the US and Netherlands to rotationally wet agricultural land can provide benefits for farming, biodiversity and flood risk management. Phase 1, which considered the general economic and legislative feasibility of the techniques in the UK, is now complete. We are now undertaking Phase 2, which considers the applicability of the techniques to specific farms around the Humber. We are looking to progress a phase 3 which would deliver some small-scale trials of the techniques. There are a number of landowners who are keen to support this work.



Techniques used in the Netherlands

Skeffling Managed Realignment Scheme: A drop-in took place on 28th June to present the embankment alignment, breach locations and flood bank level, along with further information on the types of habitat which will develop, flood protection and drainage. A 3D computer model of the scheme proved to be a valuable tool to explain the design. Following the drop-in, two workshops were held on 3rd July: one for organisations who have a technical interest in the project and a public workshop to discuss local concerns. Both workshops were well attended and the project team has gathered valuable information to further develop the design. There will be further opportunities for the local community to have their say before we submit our planning application later this year.

Humber Hull Frontage Improvements: This project is reviewing the existing defences along the north bank tidal frontage through the Hull area. The scheme will cost around £35 million. We plan to have all the necessary funding and approvals in place to enable work to start in summer 2018. Construction works will take 3 years to complete

Our visual inspection programme, surveys and updated flood modelling are complete and have indicated to us which areas of defence we need to focus on. There is still more investigation and planning to be done before any construction work can begin. We shared our plans with the local community, as part of a wider drop-in on flood risk management schemes planned for Hull, in April this year.

South Ferriby & Winteringham Ings Sea Defence Improvements: At the beginning of July, we held 2 community events to seek feedback on the shortlist of options to reduce flood risk to South Ferriby and Winteringham. Over 70 people attended, including Andrew Percy MP at the South Ferriby event. Attendees provided really valuable feedback to the Project Team. Subject to funding, works on site should begin at the end of 2018.

Are you prepared for future floods? Although flood defences reduce the likelihood of flooding, the risk can never be removed entirely.

Flooding can happen at any time and we advise people to take simple practical steps to help reduce the impact of flooding to their homes or business.

- Check your flood risk by visiting www.gov.uk/check-if-youre-at-risk-of-flooding or calling Floodline on 0345 988 1188
- If you are at risk, sign-up to our free flood warning service
- To make a flood plan for your home or business visit www.gov.uk/prepare-for-a-flood/make-a-flood-plan

Contact

If you have any questions, please contact our national customer contact team on 03708 506 506 (landlines are charged the same as a local geographic call but mobiles may vary) or email HStrategy@environment-agency.gov.uk for information about the Comprehensive Review or Humber schemes.

customer service line
03708 506 506

incident hotline
0800 80 70 60

floodline
0345 988 1188
0845 988 1188

www.gov.uk/environment-agency



Network Rail
Floor 3 George Stephenson
House
Toft Green
York
YO1 6JT
Tel No. 07730 355291
diane.cragg@networkrail.co.uk

East Riding of Yorkshire Council
Development Management
County Hall
Beverley
East Riding of Yorkshire
HU17 9BA

20th July 2017

Our ref: TP/LNE/2017-023

Sent by email

Dear Sir Madam,

Application for Prior Approval under Part 18 of the Town and Country Planning (General Permitted Development) Order 2015 for Alterations to Existing Access Arrangements on the West Side of Saltmarsh Station, Tranby Lane, Laxton

Please find enclosed a prior approval application for access alterations adjacent to Saltmarsh Station, Laxton. Prior approval is sought under Part 18 of the Town and Country Planning (General Permitted development) Order 2015 (GPDO).

The details of the access arrangements are shown on the following drawing:

Drawing no. F2G-ARP-CIV-SKE-00015 –PO1.1 – General Arrangement

Drawing no. F2G-ARP-CIV-DRG-01001 –PO2 – External works General arrangements

Drawing no. F2G-ARP-CIV-DRG-01002 –PO2 – External works 1 of 5 sheets

Plans for information only showing the entirety of the scheme of works at Saltmarsh Station; THESE PLANS ARE WORKING DRAWINGS FOR INFORMATION ONLY NOT FOR CONSIDERATION AS PART OF THE PRIOR APPROVAL APPLICATION:

Drawing no. F2G-ARP-CIV-DRG-01000 –PO1 – General Arrangement

Drawing no. F2G-ARP-CIV-DRG-01001 –PO2 – External works General arrangements

Drawing no. F2G-ARP-CIV-DRG-01002 –PO2 – External works 1 of 5 sheets

Drawing no. F2G-ARP-CIV-DRG-01003 –PO2 – External works 2 of 5 sheets

Drawing no. F2G-ARP-CIV-DRG-01004 –PO2 – External works 3 of 5 sheets
 Drawing no. F2G-ARP-CIV-DRG-01005 –PO2 – External works 4 of 5 sheets
 Drawing no. F2G-ARP-CIV-DRG-01006 –PO2 – External works 5 of 5 sheets
 Drawing no. F2G-ARP-CIV-DRG-01010 –PO2 – Platform 1 Ramp Steel work -
 General Arrangement

Saltmarshe Railway Station is on the Sheffield to Hull Line. Network Rail is in the process of undertaking the Ferriby to Gilberdyke re-signalling project. The project will provide a new signalling system controlled centrally from York Railway Operating Centre (ROC). As part of the project alterations are proposed at Saltmarshe Railway Station to remove the existing barrow crossing on safety grounds (a barrow crossing is one that allows passengers and staff to cross the line to get access to platforms). To remove the barrow crossing alternative pedestrian access is need to platform one. The proposal is to construct a ramp on land to the rear of platform one (land owned by Network Rail but until recently leased to the adjacent residential property) with a new pedestrian access from Tranby Lane. The new access will require the existing farm entrance to be modified.

The access arrangements to the highway require prior approval under Part 18 of the GPDO. The ramp and all other works proposed within and adjacent to the station are permitted under Part 18 without prior approval (detail provided for information only). Please note alterations on the south eastern side of the level crossing include the demolition of Saltmarshe Signal box; the removal of the signal box is permitted by Part 18 (Part 11 of the GPDO at B.3 says that excluded demolition means demolition on land which is deemed to be granted under Part 3 of the Act (Control over Development))

I would draw your attention to the provisions of Part 18 which states that permitted development applies when it is “authorised by (a) a local or private Act of Parliament (i.e. the original Act of Parliament that allowed the construction of the railway).....which designates specifically the nature of the development and the land upon which it may be carried out.” Development is not permitted (for specific items identified, such as buildings, bridges or alterations of an access) ...”unless the Prior Approval of the appropriate authority to the detailed plans and specifications is first obtained”. However the Prior Approval is...”not to be refused...nor are conditions to be imposed unless they are reasonably satisfied that - (a) the development....ought to be and could reasonably be carried out elsewhere on the land; or (b) the design or external appearance of any.....bridge or building....would injure the amenity of the neighbourhood and is reasonably capable of modification to avoid such injury.”

The original Act specifying this stretch of railway states that “it shall be lawful for the Company and the South Yorkshire Company from time to time to make and enter into contracts or agreements with reference to the mode of constructing and construction of any station and approaches and accommodation work. The “general

power” is provided for in Section 16 of the 1845 Railway Clauses Consolidation Act, and it is this Act which is incorporated in all railway Acts since 1845.

The decision of the Court Of Appeal in Emsley v North Eastern Railway Company (1896) confirmed that these powers are not restricted to the period of construction but can be invoked from “time to time”. The use of the original Act of Parliament as a means of using permitted development rights has also been upheld in a 2000 English Ministerial appeal decision, the reference number being APP/X/98/X5210/003059.

For the purposes of Part 18a the enabling Act in this instance is the **North-Eastern Railway Company’s (Hull and Doncaster Branch) Act 1863** incorporating the Railway Clauses Consolidation Act 1845. The extant powers apply within the limits of deviation of the Act (plan attached) which includes Saltmarshe Station and the adjacent land.

As set out in part 18 of the GPDO the test to be applied to this prior approval application is whether the development ought to be and could reasonably be carried out elsewhere on the land. Network Rail is satisfied that the new access arrangements on to Tranby Lane are acceptable and cannot reasonably be located elsewhere.

We therefore request the formal prior approval of the plans and specifications enclosed for the new access arrangements on to Tranby Lane shall be issued in accordance with the provisions of Part 18a to Schedule 2 of the Town & Country Planning (General Permitted Development) Order 2015.

I look forward to receiving confirmation of your acceptance of the attached details within time scale prescribed in article 7 of the 2015 Order.

Please note no fee is prescribed in cases of Prior Approval under Part 18 of the Order.

Please let me know if you require any further information.

Yours faithfully

Diane Cragg MRTPI
Town Planner
Network Rail

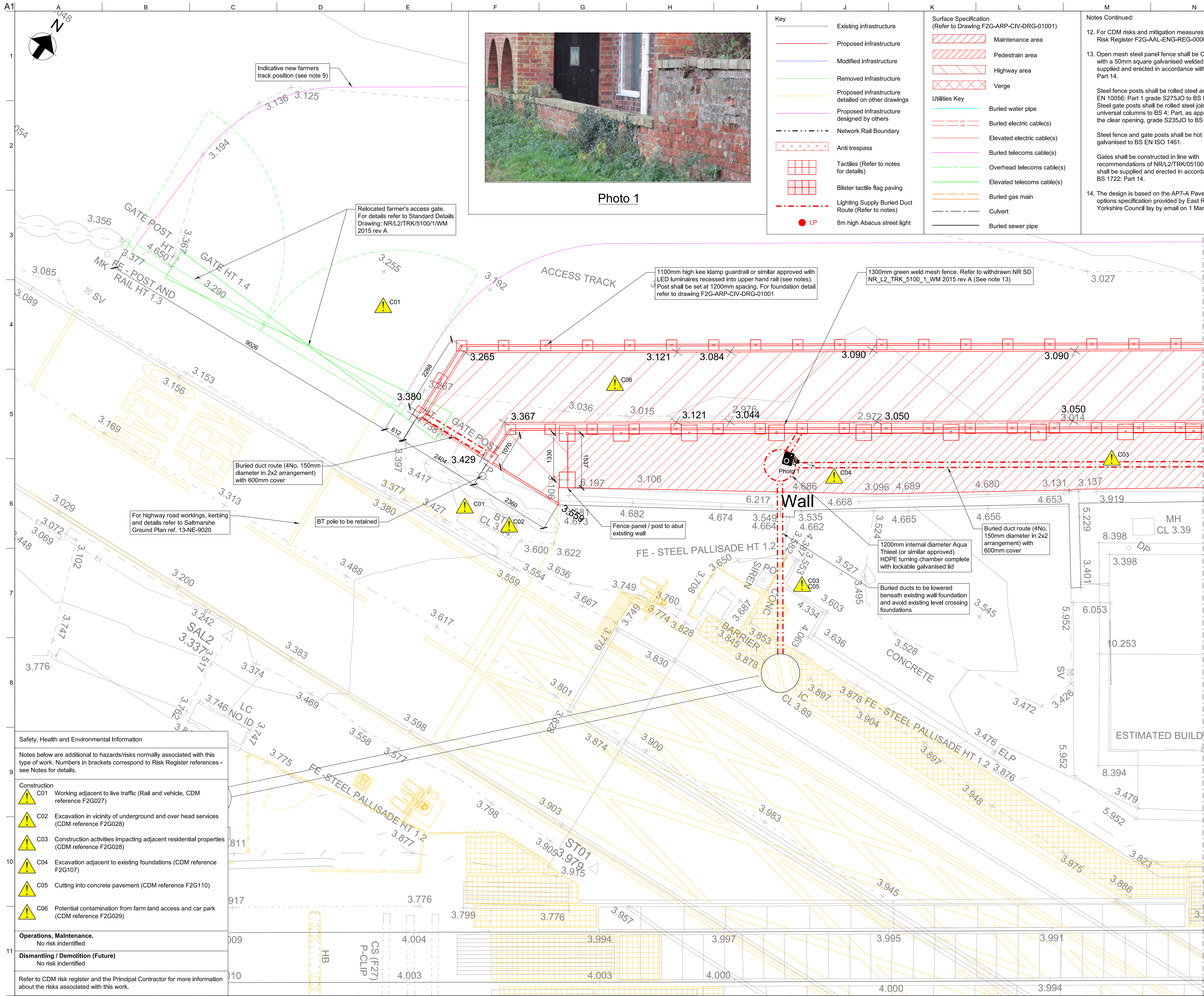


Photo 1

Key	
	Existing infrastructure
	Proposed infrastructure
	Modified infrastructure
	Removed infrastructure
	Proposed infrastructure detailed on other drawings
	Proposed infrastructure designed by others
	Network Rail Boundary
	Anti trespass
	Tactiles (Refer to notes for details)
	Blister tactile flag paving
	Lighting Supply Buried Duct Route (Refer to notes)
	8m high Abacus street light

Surface Specification (Refer to Drawing F2G-ARP-CIV-DRG-01001)	
	Maintenance area
	Pedestrian area
	Highway area
	Verge

Utilities Key	
	Buried water pipe
	Buried electric cable(s)
	Elevated electric cable(s)
	Buried telecoms cable(s)
	Overhead telecoms cable(s)
	Elevated telecoms cable(s)
	Buried gas main
	Culvert
	Buried sewer pipe

Notes Continued:

12. For CDM risks and mitigation measures refer to Risk Register F2G-AAL-ENG-REG-00002.

13. Open mesh steel panel fence shall be Category 1 with a 50mm square galvanised welded mesh supplied and erected in accordance with BS 1722: Part 14.

Steel fence posts shall be rolled steel angles to BS EN 10056: Part 1 grade S275JO to BS EN 10025. Steel gate posts shall be rolled steel joists or universal columns to BS 4: Part, as appropriate to the clear opening, grade S235JO to BS10025.

Steel fence and gate posts shall be hot dip galvanised to BS EN ISO 1461.

Gates shall be constructed in line with recommendations of NR/L2/TRK/05100. Gates shall be supplied and erected in accordance with BS 1722: Part 14.

14. The design is based on the AP7-A Pavement options specification provided by East Riding of Yorkshire Council lay by email on 1 March 2017

- Notes
- This drawings is to be read in conjunction with: Form 003 - F2G-ARP-CIV-F03-00001 NR Standard Details drawing series 400
 - All dimensions are in millimeters and all levels are in meters above ordnance datum unless stated otherwise.
 - Background mapping is reproduced from the Ordnance Survey Map with the permission of the Controller of Her Majesty's Stationary Office. © Crown Copyright. License number 0100031673.
 - The contractor must review all available utility and buried services drawings and carry out CAT scans prior to commencement of excavation and at 150mm intervals as excavation progresses to prevent accidental contact with buried services.
 - Drawing based on topographical survey undertaken by Bridgeway Consulting Ltd. from August to November 2016.
 - Land boundaries are from information provided by Network Rail in an email from Paul Spence, dated 25/08/2016, which is appended to the Form 001.
 - The demolition of the signal box and making good of land are outside the remit of this submission.
 - For Lighting requirements refer to Lighting Form A/B Drawing number F2G-ARP-EAP-DRG-00004.
 - Field access track realignment designed by others.
 - Ramp gradients and step dimensions are taken from BS 8300:5.9.2 'Design of buildings and their approaches to meet the needs of disabled people. Code of practice' and 'Design Standards for Accessible Railway Stations, Version 04, 20 March 2015'.
 - All tactile paving will be corduroy pattern unless otherwise stated and shall be laid in accordance with BS EN 7533-8:2003. 400mm x 400mm x 63mm slabs shall be laid on a 30mm sand laying course with sand filled joints. They shall be laid on at least 150mm of Type 1 sub-base at a depth to match surrounding formation level. Location of tactiles to be in accordance with NR/CIV/SD/491.
- Notes Continued.

P02	11/05/17	LC	MD	GC
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Issued for Acceptance

Rev	Date	By	Chkd	Appd
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ARUP

Blythe Gate Blythe Valley Park
Salford M6 6AE
Tel +44 121 213 3000 Fax +44 121 213 3001
www.arup.com

Client
Network Rail
Ansaldo STS UK Limited

Project Title
Ferriby to Gilberdyke
Resignalling

Key Plan

Drawing Title
Saltmarsh Station Access
External Works
Sheet 1 of 5

Scale at A1 NTS

Role
Civil Engineering

Suitability
S1 - Fit for co-ordination

Arup Job No 248307-00	Rev P02
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Name
F2G-ARP-CIV-DRG-01002

Safety, Health and Environmental Information	
Notes below are additional to hazards/risks normally associated with this type of work. Numbers in brackets correspond to Risk Register references - see Notes for details.	
	C01 Working adjacent to live traffic (Rail and vehicle, CDM reference F2G027)
	C02 Excavation in vicinity of underground and over head services (CDM reference F2G028)
	C03 Construction activities impacting adjacent residential properties (CDM reference F2G028)
	C04 Excavation adjacent to existing foundations (CDM reference F2G107)
	C05 Cutting into concrete pavement (CDM reference F2G110)
	C06 Potential contamination from farm land access and car park (CDM reference F2G029)
Operations, Maintenance, No risk identified	
Dismantling / Demolition (Future) No risk identified	
Refer to CDM risk register and the Principal Contractor for more information about the risks associated with this work.	

A1 LG

A

B

C

D

E

F

1

2

3

4

5

6

7

8

9

10

11

Do not scale

Key	Surface Specification (Refer to Drawing F2G-ARP-CIV-DRG-01001)
	Utilities Key

- Notes
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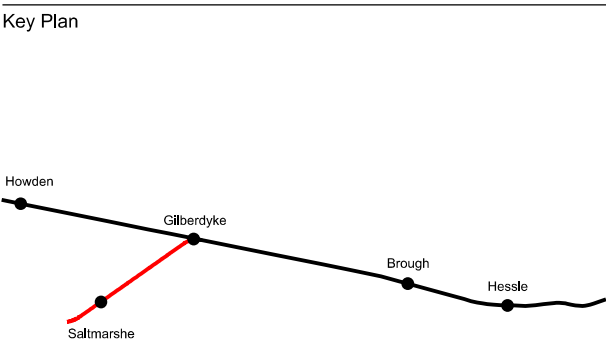
P01.1	---	---	---	---
Rev	Date	By	Chkd	Appld

ARUP

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www.arup.com

Client
Network Rail
Ansaldo STS UK Limited

Project Title
Ferryby to Gilberdyke
Resignalling



Drawing Title
Saltmarshes Station Access
General Arrangement to
Support Planning
3M 1100yds - TJG2

Scale at A1
1:100

Role
Civil Engineering

Suitability
S0 - Initial non-contractual

Arup Job No
248307-00

Name
F2G-ARP-CIV-SKE-00015

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http://wybone.co.uk/product/dog-bag-dispenser/ 17/02546/RL... Dog Bins | Li... Retriever Cit... Litter and d... Supplies We... doggie ba... New tab Dog Bag ...

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Ground Anchor Post / Ground Anchor
GAP / GA100MM **£41.49 / £111.48**
Ground Anchor Posts are designed for use with a Ground Anchor when sitting a bin into soil or soft ground. The Ground Anchor GA100MM (not included) is driven into the ground then the Ground Anchor Post can be bolted directly into the anchor.

None

Anchor Tool Kit
GATK **£73.99**
The Ground Anchor can be sited in place using our Anchor Tool Kit (GATK), which can be bought or hired (please call for details about hiring). Drive the ground anchor into soil using the fixing tool, wind out to fix into place then bolt the post to the plate. Please check for hidden cable and pipes before using this product.


3 Optional extras As Priced

Bio-degradable Dog Waste Bags
DOG/BAGS **£31.75**
Case of 800 bags that are 22 micron thick, have a deep gusset in the bottom to help with the pickup. They have a pull out strip allowing only one bag at a time to be removed.

Dog Lead Hook
DLH **£3.99**
Optional dog lead hook can be fixed to the bin giving users a handy place to secure their dog lead.

Summary

Dog Bag Dispenser



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Quantity: 1

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19:41
08/08/2017


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Dog Poop Bags
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Dog Poop Bag Dispensers
Park Dispenser Refill Bags
Dog Waste Stations
Litter Bins
Cigarette Bins
Seats & Benches
Signs & Labels
Brackets & Fixings
Accessories

Easy mounting on wall, post or dog-bin.
One-per-pull minimises wastage.
Locking front access for filling.

Specification
Size: 460mm x 160mm x 230mm
Colour: Green or Red (other colours by arrangement) Fixing options: Various Brackets are available for fixing, including walls, posts and banding



Sales price £94.80
Sales price without tax £79.00
Tax amount £15.80

Colours available Green

Price: No additional charge

1 + - [ADD TO CART](#)

Manufacturer: JRB

19:43
08/08/2017

Laxton Parish Council

Accounts for Payment

August 2017

Payee	Details	Total	VAT
Alan Bravey	Salary – August	86.45	0
Post Office	PAYE – August	58.00	0
Community Heart Beat Trust	Village Emergency Telephone Scheme Donation – 2 locations	200.00	0
Cartridge Save	Printer Ink	114.84	19.14
Laxton Victory Hall	Room Hire – 2015, 2016 and 2017	600	0
Total		1059.29	19.14